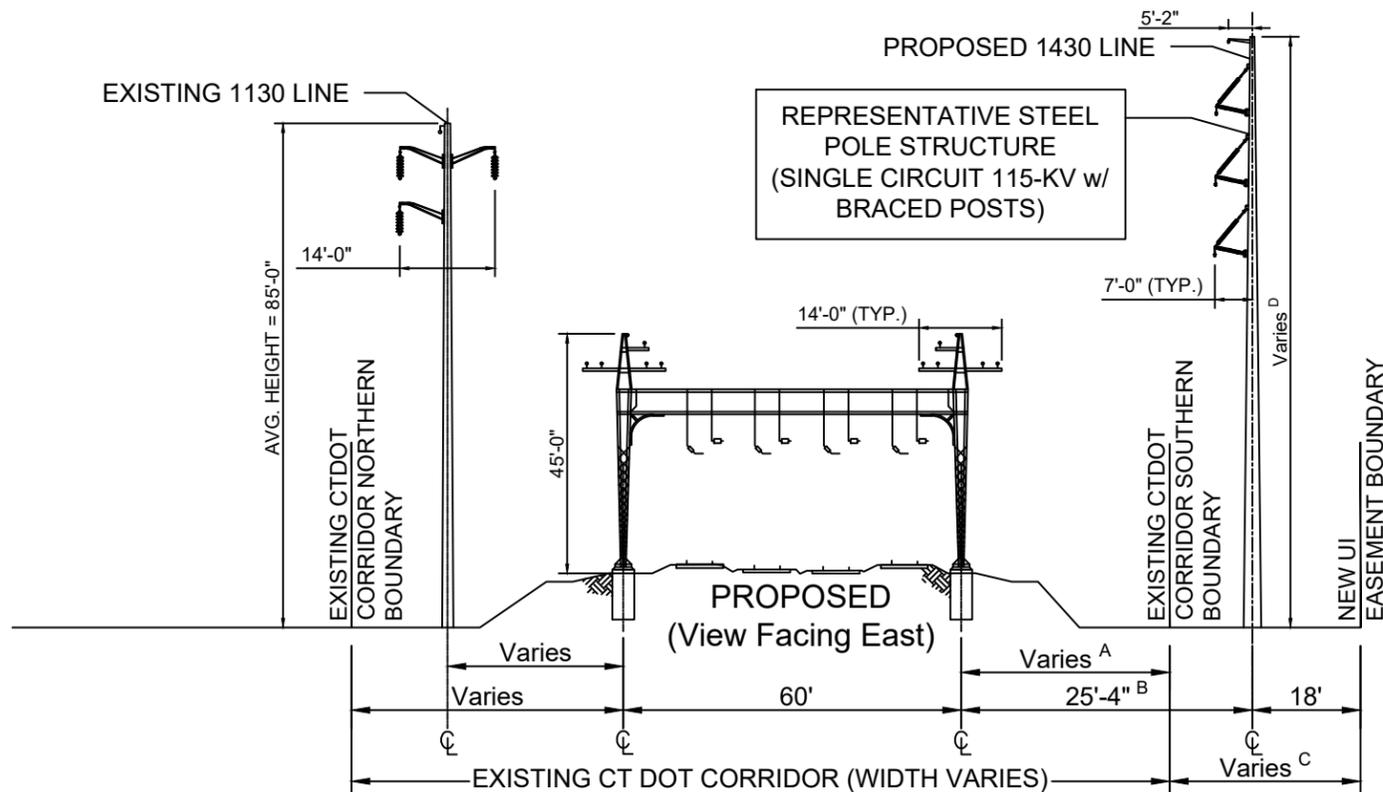
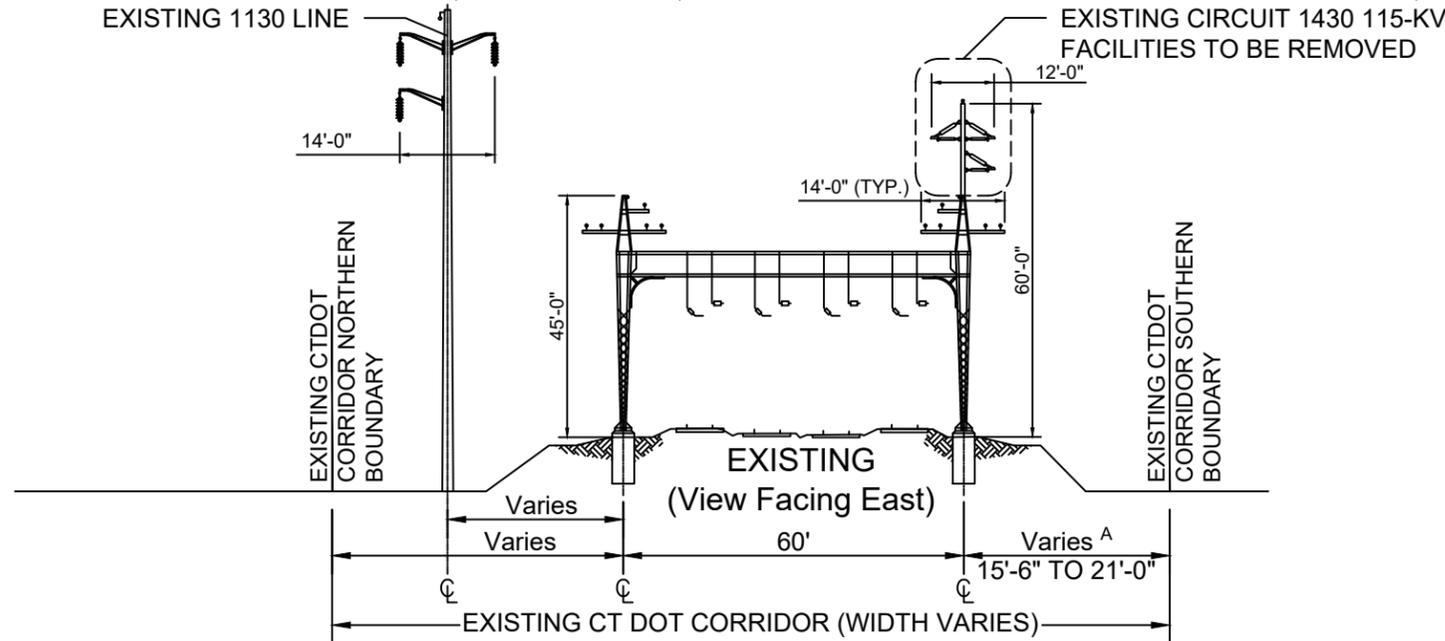




**CROSS SECTION 2**  
**EAST OF SASCO CREEK (P648S) TO ASH CREEK SUBSTATION (FAIRFIELD)**  
**STR P655S TO STR P656S - 0.14 MILES, STR P664S, STR P699S TO STR P703S - 0.30 MILES, STR P708S**

**NOTES:**

1. DEPICTED STRUCTURES ARE STEEL TANGENT STRUCTURES. THE VERTICAL ARRANGEMENT MINIMIZES CONDUCTOR BLOWOUT AND THUS, THE AMOUNT OF NEW PERMANENT UI EASEMENT REQUIRED.
2. THE EXISTING CATENARY STRUCTURES SUPPORT METRO NORTH RAILROAD ELECTRICAL FACILITIES CONSISTING UP TO TWO (2) SIGNAL WIRES, 2 OR 3 FEEDER WIRES, AND ONE (1) AERIAL GROUND WIRE.
3. EXISTING CIRCUIT 1130 STEEL POLE STRUCTURES MAY SUPPORT METRO NORTH RAILROAD ELECTRICAL FACILITIES CONSISTING UP TO TWO (2) SIGNAL WIRES AND 2 OR 3 FEEDER WIRES, AND ONE (1) AERIAL GROUND WIRE.
4. THE CT DOT CORRIDOR CONTAINS FOUR (4) RAILROAD TRACKS IN THIS AREA OPERATED BY METRO NORTH RAILROAD.
5. A 15'-0" CLEARANCE MUST BE MAINTAINED BETWEEN THE NEW 115-KV CONDUCTORS AND THE EXISTING CATENARY STRUCTURES SO THAT METRO NORTH RAILROAD CAN MAINTAIN THEIR EQUIPMENT WITHOUT REQUIRING AN OUTAGE ON THE 115-KV FACILITIES.
6. EXISTING VEGETATION WITHIN THE CT DOT CORRIDOR VARIES BY LOCATION. REFER TO ATTACHMENTS V2.3 AND V2.4: PROJECT MAPS AND DRAWINGS, FOR LOCATIONS OF PROPOSED TREE REMOVAL.
7. TO COMPLY WITH UI STANDARD TRANSMISSION VEGETATION MANAGEMENT PLANS, UI REQUIRES VEGETATION MANAGEMENT IN THE AREA THAT IS 18'-0" FROM EACH STRUCTURE CENTER (25'-0" FROM CONDUCTOR ATTACHMENT POINT). IN LOCATIONS WHERE THE CT DOT CORRIDOR BOUNDARY IS LESS THAN 18'-0" FROM THE PROPOSED STRUCTURE LOCATION, UI PROPOSES TO ACQUIRE A PERMANENT EASEMENT FROM ADJACENT LANDOWNERS. REFER TO ATTACHMENTS V2.3 AND V2.4: PROJECT MAPS AND DRAWINGS, FOR PROPOSED CLEARING AND ADDITIONAL PERMANENT EASEMENT LOCATIONS.
8. NO PROJECT ACTIVITIES WILL OCCUR ON THE EXISTING 1130 LINE WHICH IS LOCATED ON INDEPENDENT MONOPOLES ON THE NORTH SIDE OF THE METRO NOTH RAILROAD TRACKS.



<sup>A</sup> THE WIDTH OF THE EXISTING CT DOT CORRIDOR IN THE PROJECT AREA IS HIGHLY VARIABLE. REFER TO ATTACHMENTS V2.3 AND V2.4: PROJECT MAPS AND DRAWINGS, FOR CT DOT CORRIDOR BOUNDARIES.

<sup>B</sup> THIS DISTANCE AVERAGES 25'-4" BUT CAN RANGE FROM 17'-0" TO 51'-0". REFER TO THE TABLE ON SHEET 19 FOR EXACT DISTANCES.

<sup>C</sup> THE WIDTH OF UI'S REQUIRED NEW PERMANENT EASEMENT VARIES. REFER TO THE TABLE ON SHEET 19 FOR EXACT DISTANCES.

<sup>D</sup> THE HEIGHTS OF THE NEW 115-KV SINGLE-CIRCUIT STRUCTURES VARY. REFER TO THE TABLE ON SHEET 19 FOR EXACT STRUCTURE HEIGHTS.

CADD Drawing, DO NOT REVISE MANUALLY.

ANSI B

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REV.	DATE	BY	DESCRIPTION	APP.
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		SHEET 3 OF 21	
UI 115KV RAILROAD PROJECT FAIRFIELD TO CONGRESS			
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# CROSS SECTION 4

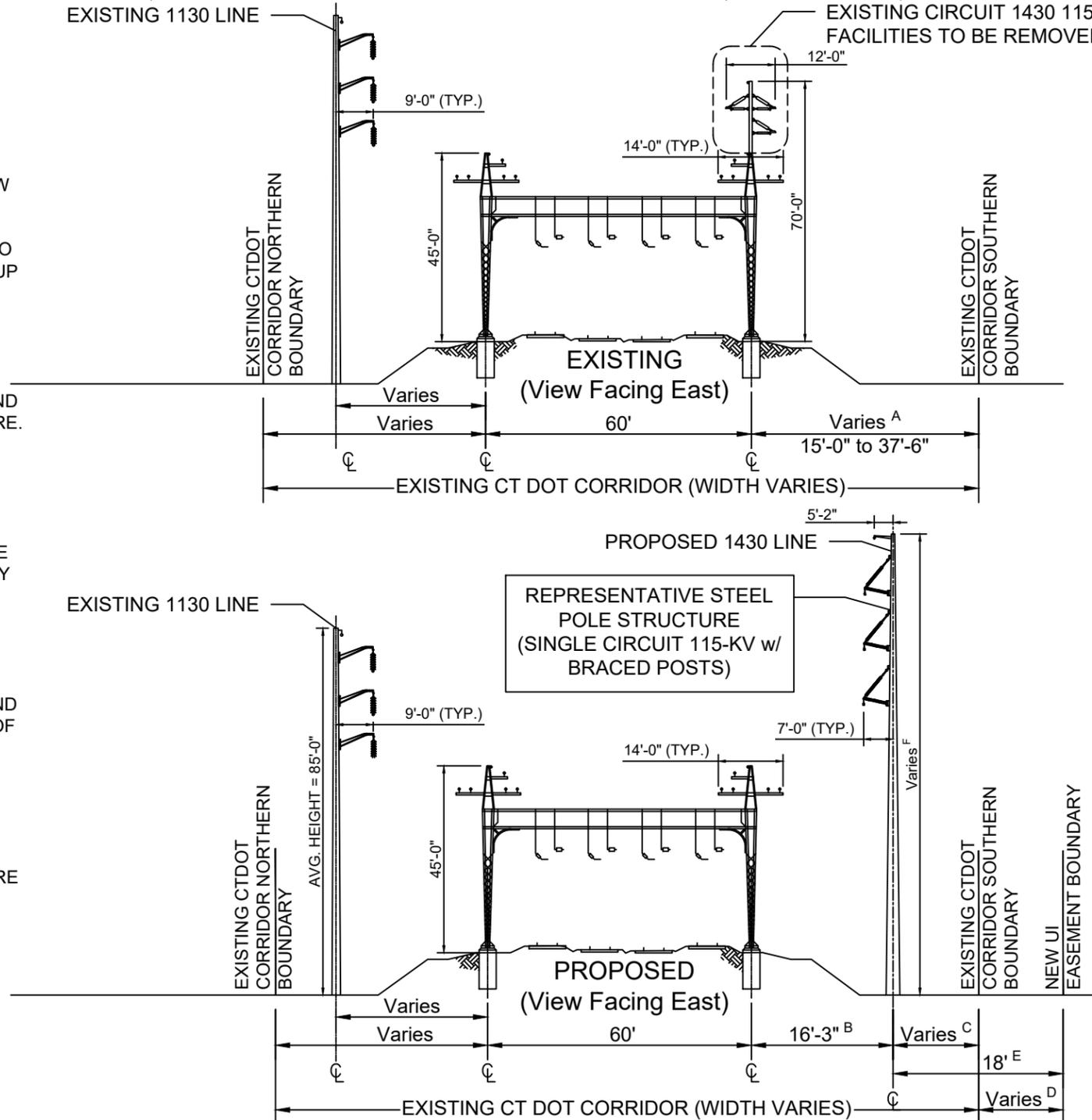
## EAST OF SASCO CREEK (P648S) TO ASH CREEK SUBSTATION (FAIRFIELD)

### STR P661S, STR P668S TO STR P671S - 0.19 MILES, STR P679S, STR P682S

EXISTING 1130 LINE EXISTING CIRCUIT 1430 115-KV FACILITIES TO BE REMOVED

**NOTES:**

1. DEPICTED STRUCTURES ARE STEEL TANGENT STRUCTURES. THE VERTICAL ARRANGEMENT MINIMIZES CONDUCTOR BLOWOUT AND THUS, THE AMOUNT OF NEW PERMANENT UI EASEMENT REQUIRED.
2. THE EXISTING CATENARY STRUCTURES SUPPORT METRO NORTH RAILROAD ELECTRICAL FACILITIES CONSISTING UP TO TWO (2) SIGNAL WIRES, 2 OR 3 FEEDER WIRES, AND ONE (1) AERIAL GROUND WIRE.
3. EXISTING CIRCUIT 1130 STEEL POLE STRUCTURES MAY SUPPORT METRO NORTH RAILROAD ELECTRICAL FACILITIES CONSISTING UP TO TWO (2) SIGNAL WIRES AND 2 OR 3 FEEDER WIRES, AND ONE (1) AERIAL GROUND WIRE.
4. THE CT DOT CORRIDOR CONTAINS FOUR (4) RAILROAD TRACKS IN THIS AREA OPERATED BY METRO NORTH RAILROAD.
5. A 15'-0" CLEARANCE MUST BE MAINTAINED BETWEEN THE NEW 115-KV CONDUCTORS AND THE EXISTING CATENARY STRUCTURES SO THAT METRO NORTH RAILROAD CAN MAINTAIN THEIR EQUIPMENT WITHOUT REQUIRING AN OUTAGE ON THE 115-KV FACILITIES.
6. EXISTING VEGETATION WITHIN THE CT DOT CORRIDOR VARIES BY LOCATION. REFER TO ATTACHMENTS V2.3 AND V2.4: PROJECT MAPS AND DRAWINGS, FOR LOCATIONS OF PROPOSED TREE REMOVAL.
7. TO COMPLY WITH UI STANDARD TRANSMISSION VEGETATION MANAGEMENT PLANS, UI REQUIRES VEGETATION MANAGEMENT IN THE AREA THAT IS 32'-0" FROM EACH STRUCTURE CENTER (25'-0" FROM CONDUCTOR ATTACHMENT POINT). IN LOCATIONS WHERE THE CT DOT CORRIDOR BOUNDARY IS GREATER THAN 32'-0" FROM THE PROPOSED STRUCTURE LOCATION, UI WILL NOT NEED ANY PERMANENT EASEMENT FROM ADJACENT LANDOWNERS TO ACHIEVE THESE CLEARANCES.
8. NO PROJECT ACTIVITIES WILL OCCUR ON THE EXISTING 1130 LINE WHICH IS LOCATED ON INDEPENDENT MONOPOLES ON THE NORTH SIDE OF THE METRO NOTH RAILROAD TRACKS.



<sup>A</sup> THE WIDTH OF THE EXISTING CT DOT CORRIDOR IN THE PROJECT AREA IS HIGHLY VARIABLE. REFER TO ATTACHMENTS V2.3 AND V2.4: PROJECT MAPS AND DRAWINGS, FOR CT DOT CORRIDOR BOUNDARIES.

<sup>B</sup> THIS DISTANCE AVERAGES 16'-3" BUT CAN RANGE FROM 10'-0" TO 25'-0". REFER TO THE TABLE ON SHEET 19 FOR EXACT DISTANCES.

<sup>C</sup> THE DISTANCE BETWEEN THE PROPOSED 115-KV SINGLE-CIRCUIT STRUCTURES AND THE EXISTING CT DOT CORRIDOR SOUTHERN BOUNDARY VARIES. REFER TO THE TABLE ON SHEET 19 FOR EXACT DISTANCES.

<sup>D</sup> THE WIDTH OF UI'S REQUIRED NEW PERMANENT EASEMENT VARIES. REFER TO THE TABLE ON SHEET 19 FOR EXACT DISTANCES.

<sup>E</sup> AT STRUCTURE P661S, THIS DISTANCE IS 21'-0". BETWEEN STRUCTURES P669S AND P671S, THIS DISTANCE IS 28'-0". REFER TO ATTACHMENTS V2.3 AND V2.4: PROJECT MAPS AND DRAWINGS, FOR PROPOSED EASEMENT BOUNDARY.

<sup>F</sup> THE HEIGHTS OF THE NEW 115-KV SINGLE-CIRCUIT STRUCTURES VARY. REFER TO THE TABLE ON SHEET 19 FOR EXACT STRUCTURE HEIGHTS.

CADD Drawing, DO NOT REVISE MANUALLY.

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		SHEET 5 OF 21			
		UI 115KV RAILROAD PROJECT FAIRFIELD TO CONGRESS			
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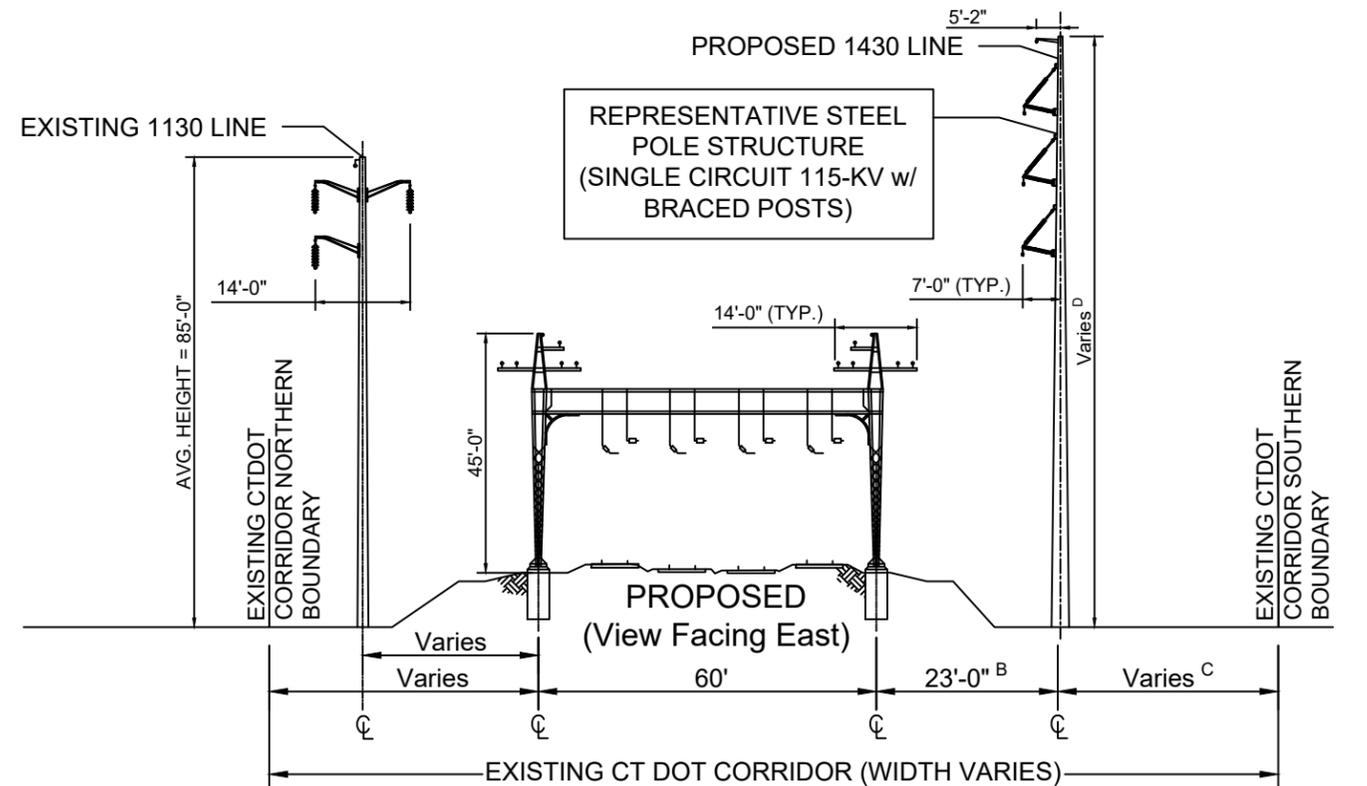
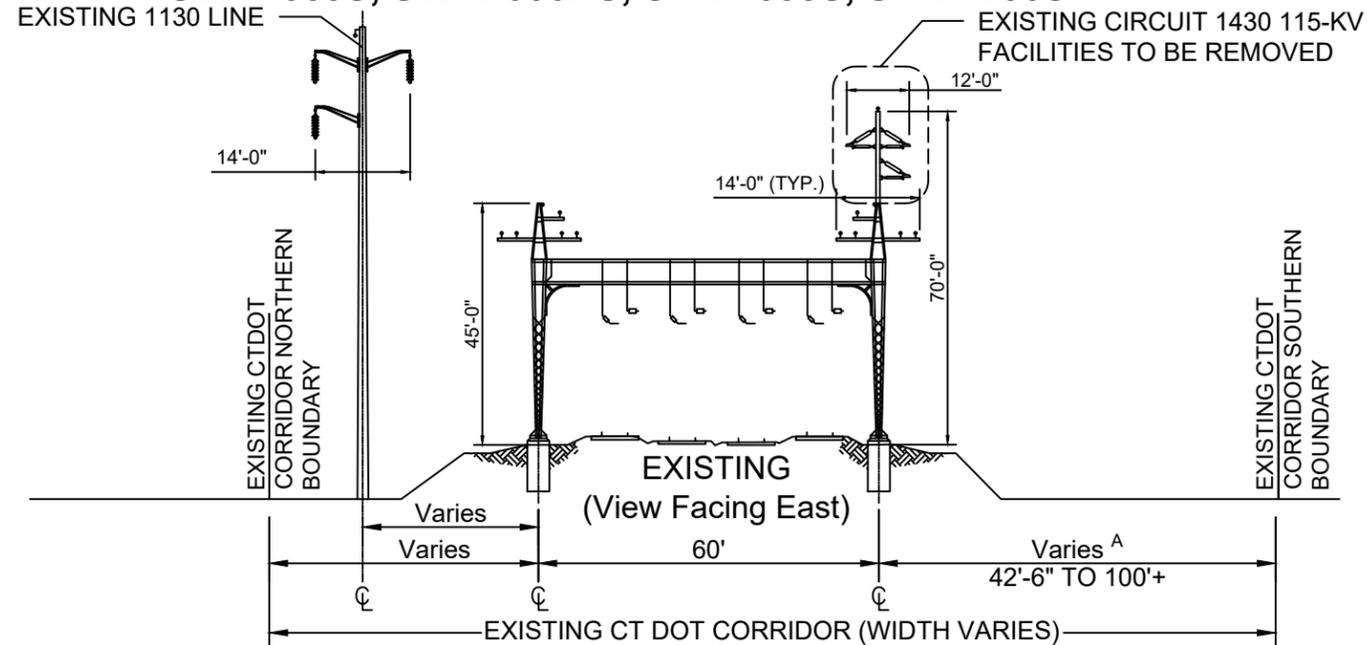
# CROSS SECTION 5

## EAST OF SASCO CREEK (P648S) TO ASH CREEK SUBSTATION (FAIRFIELD)

### STR P663S, STR P666AS, STR P688S, STR P706S

**NOTES:**

1. DEPICTED STRUCTURES ARE STEEL TANGENT STRUCTURES. THE VERTICAL ARRANGEMENT MINIMIZES CONDUCTOR BLOWOUT AND THUS, THE AMOUNT OF NEW PERMANENT UI EASEMENT REQUIRED.
- 2.
3. THE EXISTING CATENARY STRUCTURES SUPPORT METRO NORTH RAILROAD ELECTRICAL FACILITIES CONSISTING UP TO TWO (2) SIGNAL WIRES, 2 OR 3 FEEDER WIRES, AND ONE (1) AERIAL GROUND WIRE.
4. EXISTING CIRCUIT 1130 STEEL POLE STRUCTURES MAY SUPPORT METRO NORTH RAILROAD ELECTRICAL FACILITIES CONSISTING UP TO TWO (2) SIGNAL WIRES AND 2 OR 3 FEEDER WIRES, AND ONE (1) AERIAL GROUND WIRE.
5. THE CT DOT CORRIDOR CONTAINS FOUR (4) RAILROAD TRACKS IN THIS AREA OPERATED BY METRO NORTH RAILROAD.
6. A 15'-0" CLEARANCE MUST BE MAINTAINED BETWEEN THE NEW 115-KV CONDUCTORS AND THE EXISTING CATENARY STRUCTURES SO THAT METRO NORTH RAILROAD CAN MAINTAIN THEIR EQUIPMENT WITHOUT REQUIRING AN OUTAGE ON THE 115-KV FACILITIES.
7. EXISTING VEGETATION WITHIN THE CT DOT CORRIDOR VARIES BY LOCATION. REFER TO ATTACHMENTS V2.3 AND V2.4: PROJECT MAPS AND DRAWINGS, FOR LOCATIONS OF PROPOSED TREE REMOVAL.
8. TO COMPLY WITH UI STANDARD TRANSMISSION VEGETATION MANAGEMENT PLANS, UI REQUIRES VEGETATION MANAGEMENT IN THE AREA THAT IS 18'-0" FROM EACH STRUCTURE CENTER (25'-0" FROM CONDUCTOR ATTACHMENT POINT). IN LOCATIONS WHERE THE CT DOT CORRIDOR BOUNDARY IS GREATER THAN 18'-0" FROM THE PROPOSED STRUCTURE LOCATION, UI WILL NOT NEED ANY PERMANENT EASEMENT FROM ADJACENT LANDOWNERS TO ACHIEVE THESE CLEARANCES.
9. NO PROJECT ACTIVITIES WILL OCCUR ON THE EXISTING 1130 LINE WHICH IS LOCATED ON INDEPENDENT MONOPOLES ON THE NORTH SIDE OF THE METRO NOTH RAILROAD TRACKS.



<sup>A</sup> THE WIDTH OF THE EXISTING CT DOT CORRIDOR IN THE PROJECT AREA IS HIGHLY VARIABLE. REFER TO ATTACHMENTS V2.3 AND V2.4: PROJECT MAPS AND DRAWINGS, FOR CT DOT CORRIDOR BOUNDARIES.

<sup>B</sup> THIS DISTANCE AVERAGES 23'-0" BUT CAN RANGE FROM 13'-0" TO 50'-0". REFER TO THE TABLE ON SHEET 19 FOR EXACT DISTANCES.

<sup>C</sup> THE DISTANCE BETWEEN THE PROPOSED 115-KV SINGLE-CIRCUIT STRUCTURES AND THE EXISTING CT DOT CORRIDOR SOUTHERN BOUNDARY VARIES. REFER TO THE TABLE ON SHEET 19 FOR EXACT DISTANCES.

<sup>D</sup> THE HEIGHTS OF THE NEW 115-KV SINGLE-CIRCUIT STRUCTURES VARY. REFER TO THE TABLE ON SHEET 19 FOR EXACT STRUCTURE HEIGHTS.

ANSI B CADD Drawing, DO NOT REVISE MANUALLY.

**TYPICAL CROSS SECTION DIAGRAMS**

SHEET 6 OF 21

**UI 115KV RAILROAD PROJECT FAIRFIELD TO CONGRESS**

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REV.	DATE	BY	DESCRIPTION	APP.
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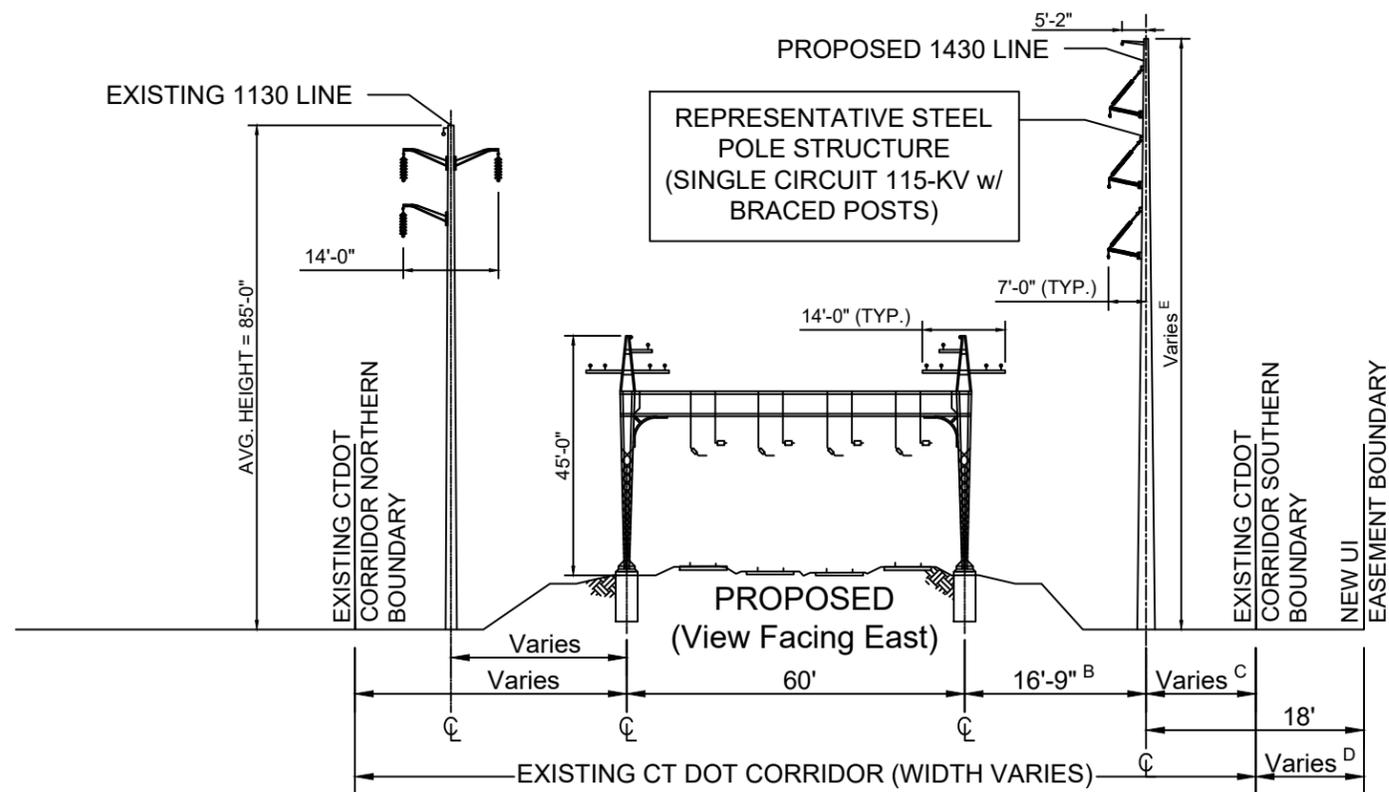
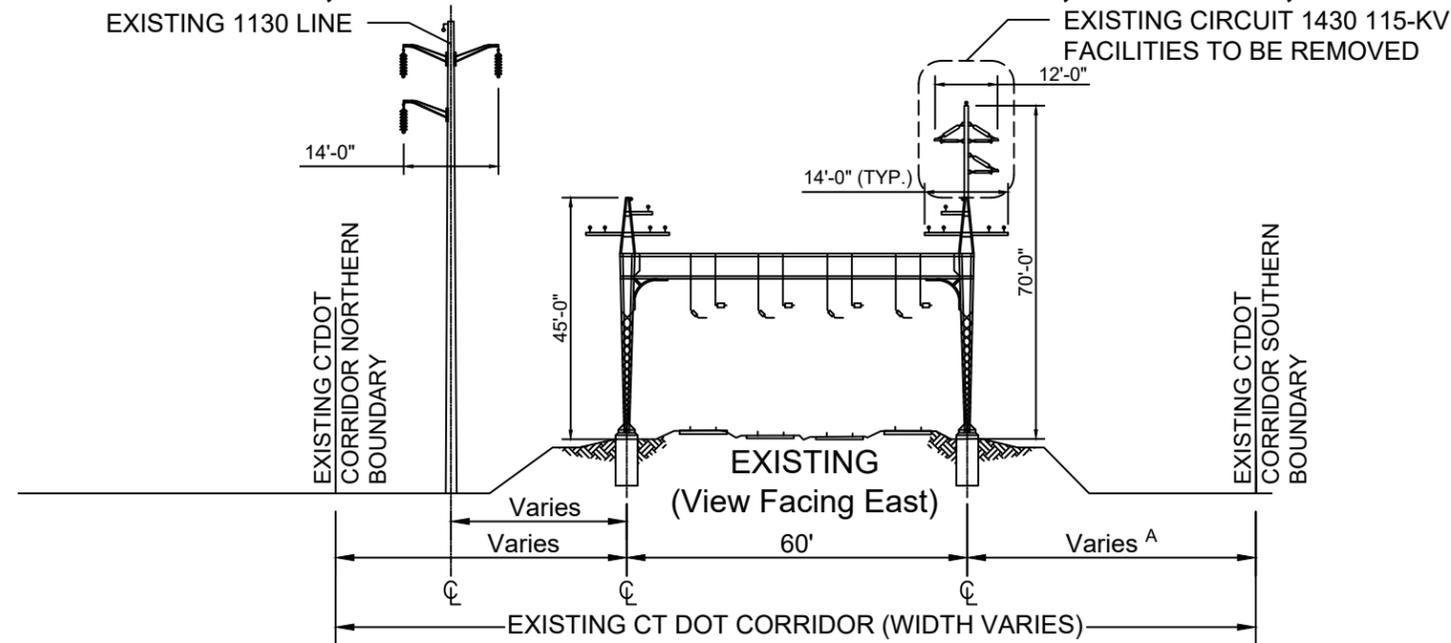
## CROSS SECTION 6

### EAST OF SASCO CREEK (P648S) TO ASH CREEK SUBSTATION (FAIRFIELD)

**STR P665BS, STR P673S TO STR P678S - 0.45 MILES, STR P689S TO STR P698S - 0.53 MILES, STR P704S, STR P709S TO STR P713S - 0.28 MILES**

**NOTES:**

1. DEPICTED STRUCTURES ARE STEEL TANGENT STRUCTURES. THE VERTICAL ARRANGEMENT MINIMIZES CONDUCTOR BLOWOUT AND THUS, THE AMOUNT OF NEW PERMANENT UI EASEMENT REQUIRED.
2. THE EXISTING CATENARY STRUCTURES SUPPORT METRO NORTH RAILROAD ELECTRICAL FACILITIES CONSISTING UP TO TWO (2) SIGNAL WIRES, 2 OR 3 FEEDER WIRES, AND ONE (1) AERIAL GROUND WIRE.
3. EXISTING CIRCUIT 1130 STEEL POLE STRUCTURES MAY SUPPORT METRO NORTH RAILROAD ELECTRICAL FACILITIES CONSISTING UP TO TWO (2) SIGNAL WIRES AND 2 OR 3 FEEDER WIRES, AND ONE (1) AERIAL GROUND WIRE.
4. THE CT DOT CORRIDOR CONTAINS FOUR (4) RAILROAD TRACKS IN THIS AREA OPERATED BY METRO NORTH RAILROAD.
5. A 15'-0" CLEARANCE MUST BE MAINTAINED BETWEEN THE NEW 115-KV CONDUCTORS AND THE EXISTING CATENARY STRUCTURES SO THAT METRO NORTH RAILROAD CAN MAINTAIN THEIR EQUIPMENT WITHOUT REQUIRING AN OUTAGE ON THE 115-KV FACILITIES.
6. EXISTING VEGETATION WITHIN THE CT DOT CORRIDOR VARIES BY LOCATION. REFER TO ATTACHMENTS V2.3 AND V2.4: PROJECT MAPS AND DRAWINGS, FOR LOCATIONS OF PROPOSED TREE REMOVAL.
7. TO COMPLY WITH UI STANDARD TRANSMISSION VEGETATION MANAGEMENT PLANS, UI REQUIRES VEGETATION MANAGEMENT IN THE AREA THAT IS 18'-0" FROM EACH STRUCTURE CENTER (25'-0" FROM CONDUCTOR ATTACHMENT POINT). IN LOCATIONS WHERE THE CT DOT CORRIDOR BOUNDARY IS LESS THAN 18'-0" FROM THE PROPOSED STRUCTURE LOCATION, UI PROPOSES TO ACQUIRE A PERMANENT EASEMENT FROM ADJACENT LANDOWNERS TO ACHIEVE THESE CLEARANCES.
8. NO PROJECT ACTIVITIES WILL OCCUR ON THE EXISTING 1130 LINE WHICH IS LOCATED ON INDEPENDENT MONOPOLES ON THE NORTH SIDE OF THE METRO NOTH RAILROAD TRACKS.



<sup>A</sup> THE WIDTH OF THE EXISTING CT DOT CORRIDOR IN THE PROJECT AREA IS HIGHLY VARIABLE. REFER TO ATTACHMENTS V2.3 AND V2.4: PROJECT MAPS AND DRAWINGS, FOR CT DOT CORRIDOR BOUNDARIES.

<sup>B</sup> THIS DISTANCE AVERAGES 16'-9" BUT CAN RANGE FROM 10'-0" TO 38'-0". REFER TO THE TABLE ON SHEET 19 FOR EXACT DISTANCES.

<sup>C</sup> THE DISTANCE BETWEEN THE PROPOSED 115-KV SINGLE-CIRCUIT STRUCTURES AND THE EXISTING CT DOT CORRIDOR SOUTHERN BOUNDARY VARIES. REFER TO THE TABLE ON SHEET 19 FOR EXACT DISTANCES.

<sup>D</sup> THE WIDTH OF UI'S REQUIRED PERMANENT EASEMENT VARIES. REFER TO THE TABLE ON SHEET 19 FOR EXACT DISTANCES.

<sup>E</sup> THE HEIGHTS OF THE NEW 115-KV SINGLE-CIRCUIT STRUCTURES VARY. REFER TO THE TABLE ON SHEET 19 FOR EXACT STRUCTURE HEIGHTS.

CADD Drawing, DO NOT REVISE MANUALLY.

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TYPICAL CROSS SECTION DIAGRAMS																													
SHEET 7 OF 21																													
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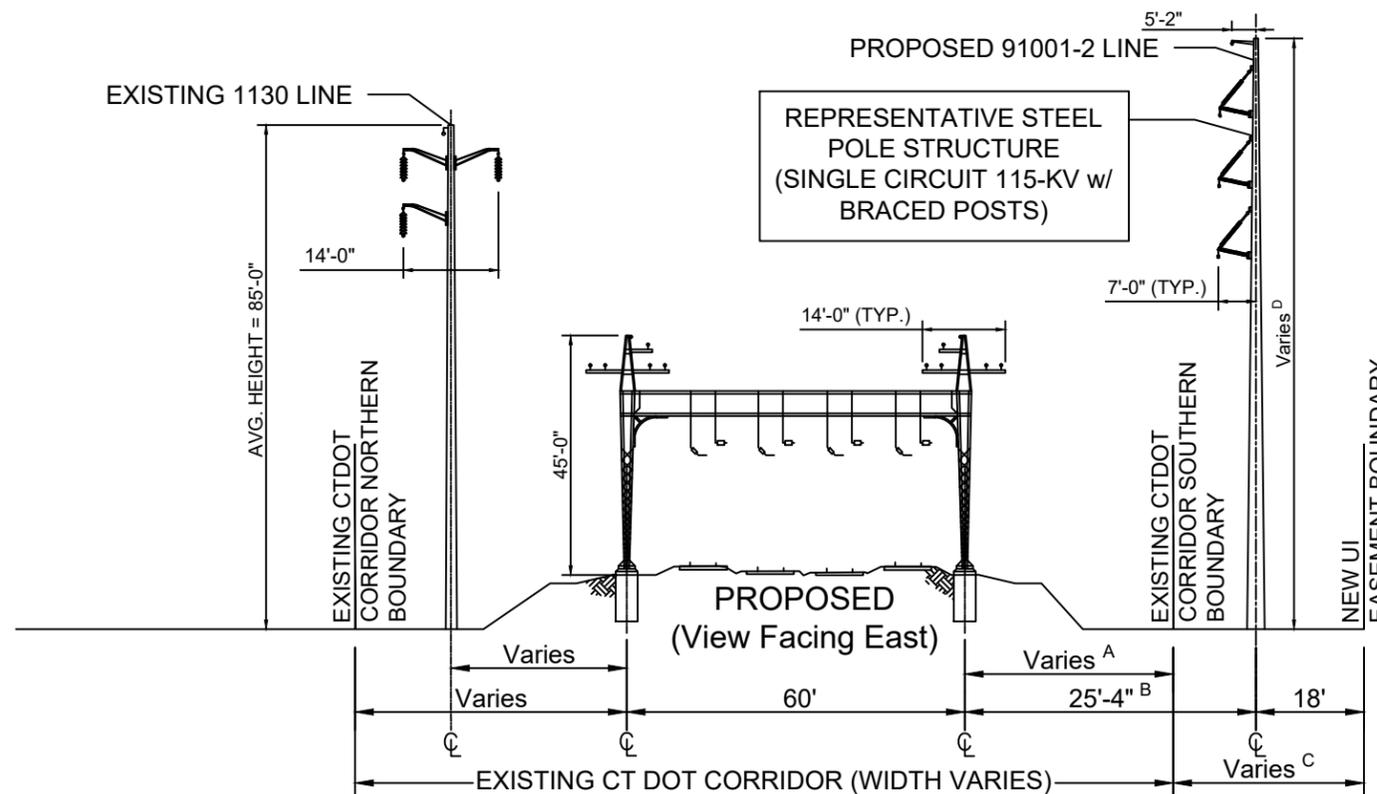
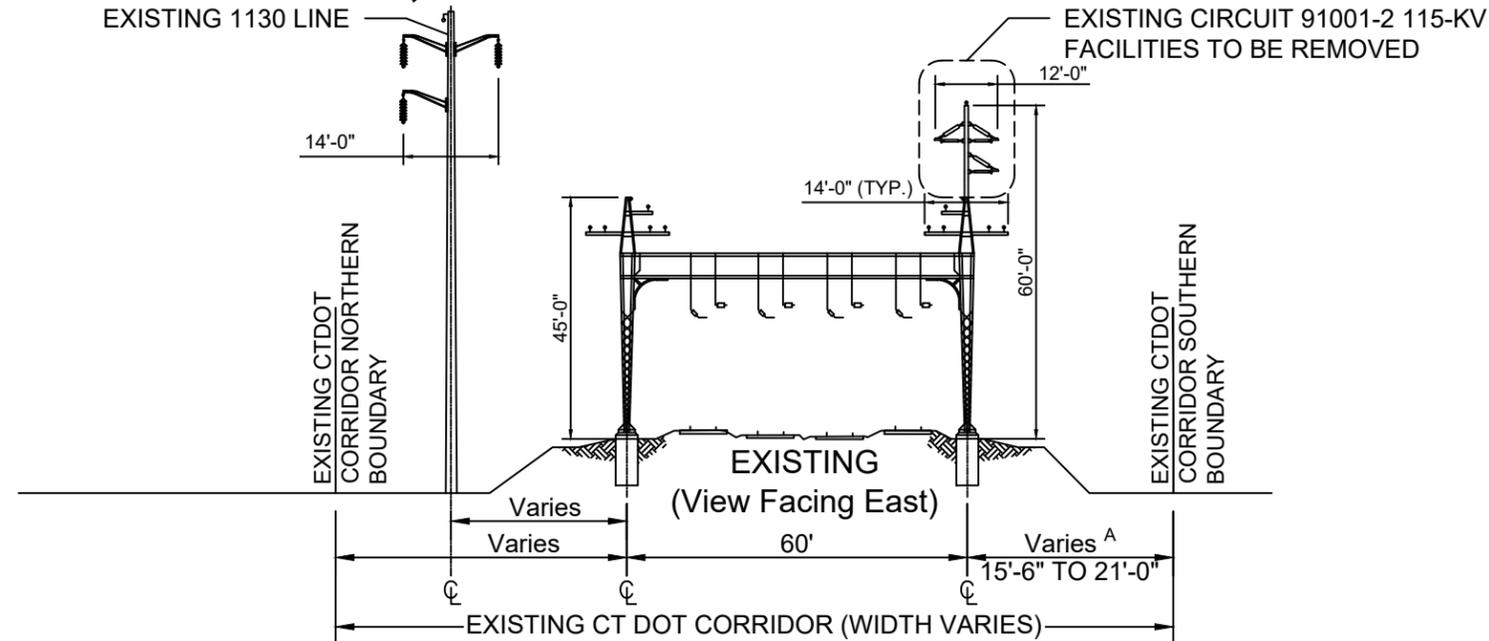
# CROSS SECTION 9

## ASH CREEK SUBSTATION TO PEQUONNOCK SUBSTATION (BRIDGEPORT)

### STR P716S, STR P721ES TO STR P724S - 0.22 MILES

**NOTES:**

1. DEPICTED STRUCTURES ARE STEEL TANGENT STRUCTURES. THE VERTICAL ARRANGEMENT MINIMIZES CONDUCTOR BLOWOUT AND THUS, THE AMOUNT OF NEW PERMANENT UI EASEMENT REQUIRED.
2. THE EXISTING CATENARY STRUCTURES SUPPORT METRO NORTH RAILROAD ELECTRICAL FACILITIES CONSISTING UP TO TWO (2) SIGNAL WIRES, 2 OR 3 FEEDER WIRES, AND ONE (1) AERIAL GROUND WIRE.
3. EXISTING CIRCUIT 1130 STEEL POLE STRUCTURES MAY SUPPORT METRO NORTH RAILROAD ELECTRICAL FACILITIES CONSISTING UP TO TWO (2) SIGNAL WIRES AND 2 OR 3 FEEDER WIRES, AND ONE (1) AERIAL GROUND WIRE.
4. THE CT DOT CORRIDOR CONTAINS FOUR (4) RAILROAD TRACKS IN THIS AREA OPERATED BY METRO NORTH RAILROAD.
5. A 15'-0" CLEARANCE MUST BE MAINTAINED BETWEEN THE NEW 115-KV CONDUCTORS AND THE EXISTING CATENARY STRUCTURES SO THAT METRO NORTH RAILROAD CAN MAINTAIN THEIR EQUIPMENT WITHOUT REQUIRING AN OUTAGE ON THE 115-KV FACILITIES.
6. EXISTING VEGETATION WITHIN THE CT DOT CORRIDOR VARIES BY LOCATION. REFER TO ATTACHMENTS V2.3 AND V2.4: PROJECT MAPS AND DRAWINGS, FOR LOCATIONS OF PROPOSED TREE REMOVAL.
7. TO COMPLY WITH UI STANDARD TRANSMISSION VEGETATION MANAGEMENT PLANS, UI REQUIRES VEGETATION MANAGEMENT IN THE AREA THAT IS 18'-0" FROM EACH STRUCTURE CENTER (25'-0" FROM CONDUCTOR ATTACHMENT POINT). IN LOCATIONS WHERE THE CT DOT CORRIDOR BOUNDARY IS LESS THAN 18'-0" FROM THE PROPOSED STRUCTURE LOCATION, UI PROPOSES TO ACQUIRE A PERMANENT EASEMENT FROM ADJACENT LANDOWNERS. REFER TO ATTACHMENTS V2.3 AND V2.4: PROJECT MAPS AND DRAWINGS, FOR PROPOSED CLEARING AND ADDITIONAL PERMANENT EASEMENT LOCATIONS.
8. EXISTING STEEL POLE ON NORTH SIDE OF METRO NORTH RAILROAD TRACKS WILL REMAIN UNCHANGED FROM EXISTING CONDITIONS.



<sup>A</sup> THE WIDTH OF THE EXISTING CT DOT CORRIDOR IN THE PROJECT AREA IS HIGHLY VARIABLE. REFER TO ATTACHMENTS V2.3 AND V2.4: PROJECT MAPS AND DRAWINGS, FOR CT DOT CORRIDOR BOUNDARIES.

<sup>B</sup> THIS DISTANCE AVERAGES 25'-4" BUT CAN RANGE FROM 17'-0" TO 51'-0". REFER TO THE TABLE ON SHEET 19 FOR EXACT DISTANCES.

<sup>C</sup> THE WIDTH OF UI'S REQUIRED NEW PERMANENT EASEMENT VARIES. REFER TO THE TABLE ON SHEET 19 FOR EXACT DISTANCES.

<sup>D</sup> THE HEIGHTS OF THE NEW 115-KV SINGLE-CIRCUIT STRUCTURES VARY. REFER TO THE TABLE ON SHEET 19 FOR EXACT STRUCTURE HEIGHTS.

CADD Drawing, DO NOT REVISE MANUALLY.

ANSI B

		TYPICAL CROSS SECTION DIAGRAMS					
						SHEET 10 OF 21	
						UI 115KV RAILROAD PROJECT FAIRFIELD TO CONGRESS	
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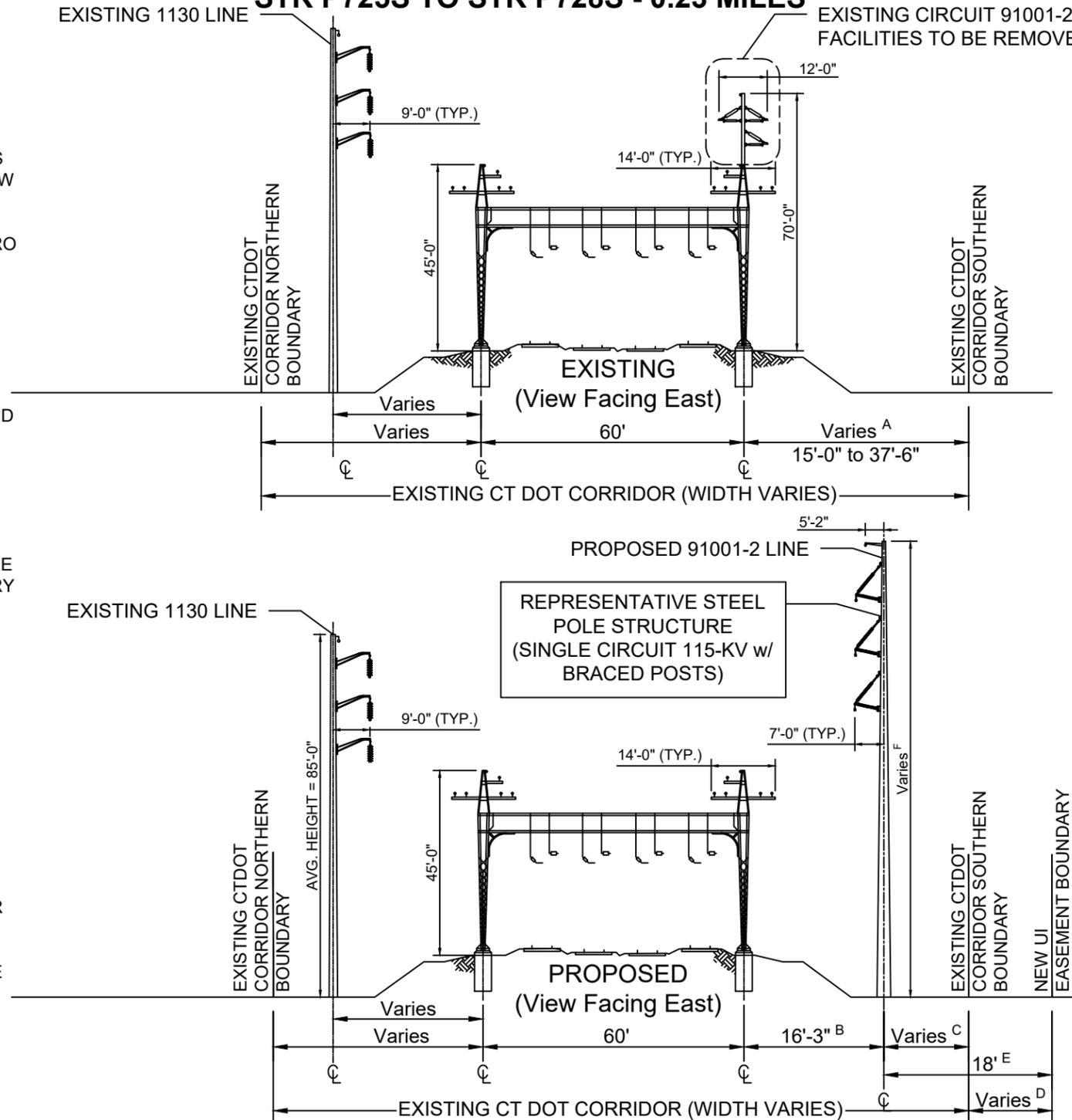
# CROSS SECTION 11 ASH CREEK SUBSTATION TO PEQUONNOCK SUBSTATION (BRIDGEPORT)

## STR P725S TO STR P728S - 0.23 MILES

EXISTING 1130 LINE EXISTING CIRCUIT 91001-2 115-KV FACILITIES TO BE REMOVED

**NOTES:**

1. DEPICTED STRUCTURES ARE STEEL TANGENT STRUCTURES. THE VERTICAL ARRANGEMENT MINIMIZES CONDUCTOR BLOWOUT AND THUS, THE AMOUNT OF NEW PERMANENT UI EASEMENT REQUIRED.
2. THE EXISTING CATENARY STRUCTURES SUPPORT METRO NORTH RAILROAD ELECTRICAL FACILITIES CONSISTING UP TO TWO (2) SIGNAL WIRES, 2 OR 3 FEEDER WIRES, AND ONE (1) AERIAL GROUND WIRE.
3. EXISTING CIRCUIT 1130 STEEL POLE STRUCTURES MAY SUPPORT METRO NORTH RAILROAD ELECTRICAL FACILITIES CONSISTING UP TO TWO (2) SIGNAL WIRES AND 2 OR 3 FEEDER WIRES, AND ONE (1) AERIAL GROUND WIRE
4. THE CT DOT CORRIDOR CONTAINS FOUR (4) RAILROAD TRACKS IN THIS AREA OPERATED BY METRO NORTH RAILROAD.
5. A 15'-0" CLEARANCE MUST BE MAINTAINED BETWEEN THE NEW 115-KV CONDUCTORS AND THE EXISTING CATENARY STRUCTURES SO THAT METRO NORTH RAILROAD CAN MAINTAIN THEIR EQUIPMENT WITHOUT REQUIRING AN OUTAGE ON THE 115-KV FACILITIES.
6. EXISTING VEGETATION WITHIN THE CT DOT CORRIDOR VARIES BY LOCATION. REFER TO ATTACHMENTS V2.3 AND V2.4: PROJECT MAPS AND DRAWINGS, FOR LOCATIONS OF PROPOSED TREE REMOVAL.
7. TO COMPLY WITH UI STANDARD TRANSMISSION VEGETATION MANAGEMENT PLANS, UI REQUIRES VEGETATION MANAGEMENT IN THE AREA THAT IS 32'-0" FROM EACH STRUCTURE CENTER (25'-0" FROM CONDUCTOR ATTACHMENT POINT). IN LOCATIONS WHERE THE CT DOT CORRIDOR BOUNDARY IS GREATER THAN 32'-0" FROM THE PROPOSED STRUCTURE LOCATION, UI WILL NOT NEED ANY PERMANENT EASEMENT FROM ADJACENT LANDOWNERS TO ACHIEVE THESE CLEARANCES.
8. NO PROJECT ACTIVITIES WILL OCCUR ON THE EXISTING 1130 LINE WHICH IS LOCATED ON INDEPENDENT MONOPOLES ON THE NORTH SIDE OF THE METRO NOTH RAILROAD TRACKS.



<sup>A</sup> THE WIDTH OF THE EXISTING CT DOT CORRIDOR IN THE PROJECT AREA IS HIGHLY VARIABLE. REFER TO ATTACHMENTS V2.3 AND V2.4: PROJECT MAPS AND DRAWINGS, FOR CT DOT CORRIDOR BOUNDARIES.

<sup>B</sup> THIS DISTANCE AVERAGES 16'-3" BUT CAN RANGE FROM 10'-0" TO 25'-0". REFER TO THE TABLE ON SHEET 19 FOR EXACT DISTANCES.

<sup>C</sup> THE DISTANCE BETWEEN THE PROPOSED 115-KV SINGLE-CIRCUIT STRUCTURES AND THE EXISTING CT DOT CORRIDOR SOUTHERN BOUNDARY VARIES. REFER TO THE TABLE ON SHEET 19 FOR EXACT DISTANCES.

<sup>D</sup> THE WIDTH OF UI'S REQUIRED NEW PERMANENT EASEMENT VARIES. REFER TO THE TABLE ON SHEET 19 FOR EXACT DISTANCES.

<sup>E</sup> AT STRUCTURE P661S, THIS DISTANCE IS 21'-0". BETWEEN STRUCTURES P669S AND P671S, THIS DISTANCE IS 28'-0". REFER TO THE ATTACHMENTS V2.3 AND V2.4: PROJECT MAPS AND DRAWINGS, FOR PROPOSED UI EASEMENT BOUNDARY.

<sup>F</sup> THE HEIGHTS OF THE NEW 115-KV SINGLE-CIRCUIT STRUCTURES VARY. REFER TO THE TABLE ON SHEET 19 FOR EXACT STRUCTURE HEIGHTS.

CADD Drawing, DO NOT REVISE MANUALLY.

ANSI B

PE Stamp		TYPICAL CROSS SECTION DIAGRAMS			
		SHEET 12 OF 21			
		UI 115KV RAILROAD PROJECT FAIRFIELD TO CONGRESS			
		DR.	ASW	SCALE: NTS	FILE:
		CK.	MSP	NO.	REV.
		APP.	--	XS-11	0-0C
		DATE:	4/15/2022		
REV.	DATE	BY	DESCRIPTION	APP.	
0-0C	8/19/2022	WESTWOOD	ISSUE FOR REVIEW	MSP	
0-0B	06/24/2022	WESTWOOD	ISSUE FOR REVIEW	MSP	
0-0A	4/15/2022	WESTWOOD	ISSUE FOR REVIEW	MSP	





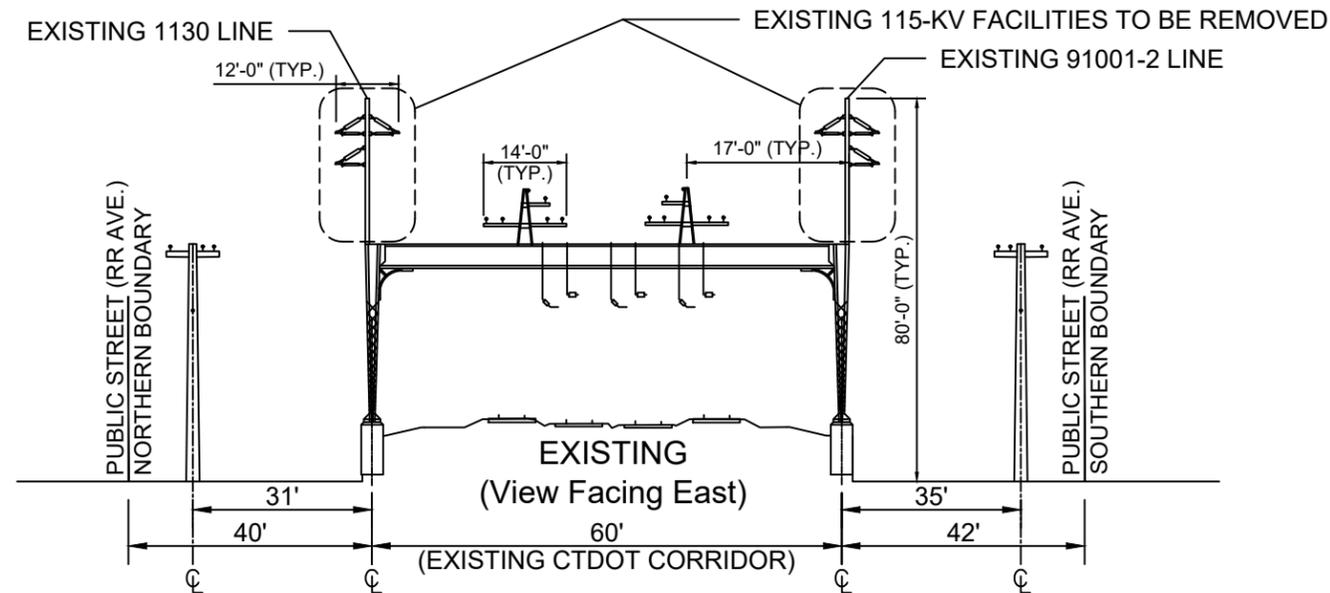
# CROSS SECTION 14

## ASH CREEK SUBSTATION TO PEQUONNOCK SUBSTATION (BRIDGEPORT)

### STR P745S TO STR P752S - 0.40 MILES, STR P762S TO STR P765AS - 0.15 MILES

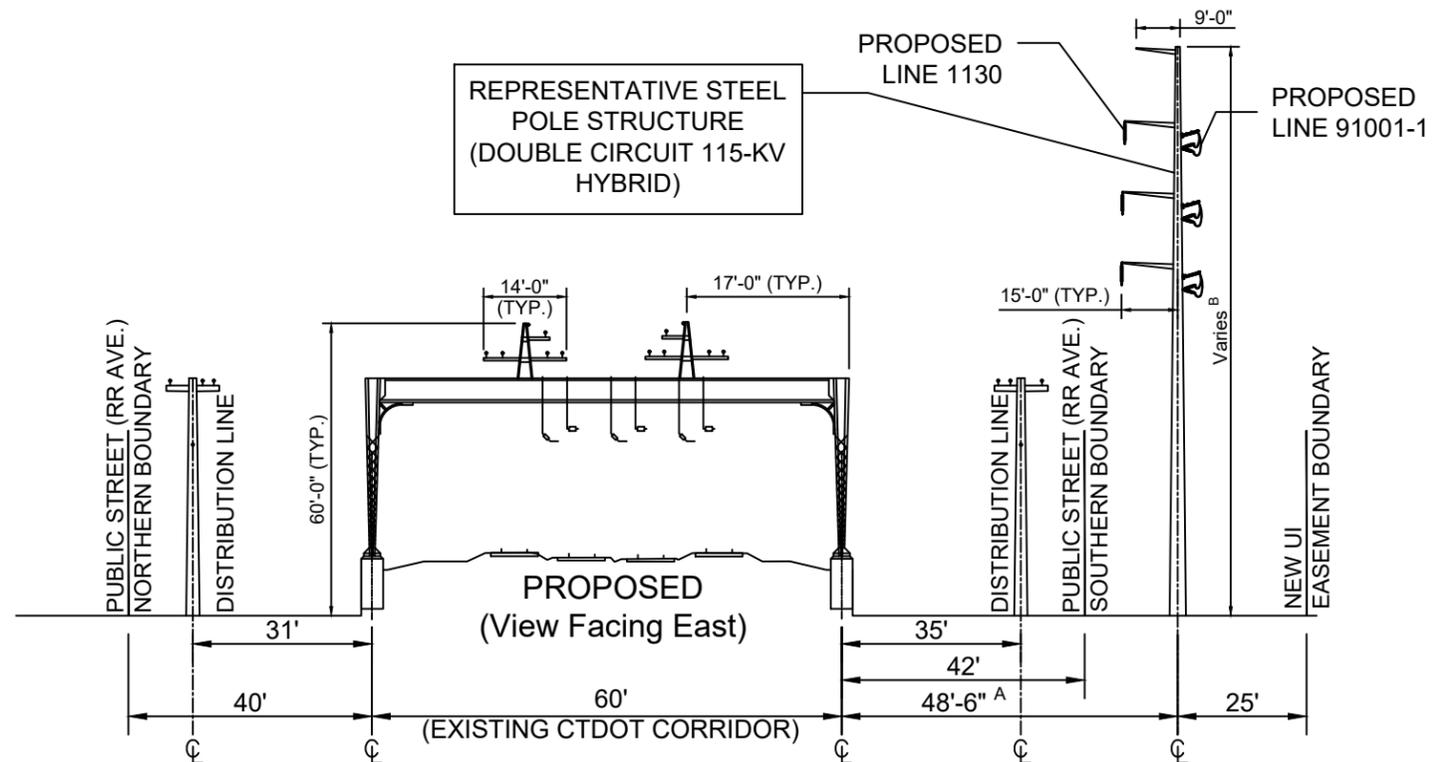
**NOTES:**

1. DEPICTED STRUCTURES ARE STEEL HYBRID (DEADEND/SUSPENSION) STRUCTURES. TANGENT AND DEADEND STRUCTURES WILL DIFFER.
2. THE EXISTING CATENARY STRUCTURES SUPPORT METRO NORTH RAILROAD ELECTRICAL FACILITIES CONSISTING OF TWO (2) SIGNAL WIRES, 2 OR 3 FEEDER WIRES, AND ONE (1) AERIAL GROUND WIRE, INSET 17' FROM BOTH THE NORTHERN AND SOUTHERN CATENARY SUPPORT COLUMNS.
3. THE CT DOT CORRIDOR CONTAINS FOUR (4) ELEVATED RAILROAD TRACKS IN THIS AREA OPERATED BY METRO NORTH RAILROAD, AND IS BOUND ON BOTH NORTH AND SOUTH SIDES BY A PUBLIC STREET (RAILROAD AVENUE.)
4. TO MAINTAIN A.D.A. COMPLIANCE IN CITY SIDEWALKS, IT WAS DEEMED NECESSARY TO PLACE THE PROPOSED MONOPOLES ON PRIVATE PROPERTY, SOUTH OF THE SIDEWALK AND EXISTING UI DISTRIBUTION LINE.
5. UI DISTRIBUTION LINES ON BOTH THE NORTH SIDE AND THE SOUTH SIDE EXIST WITHIN BRIDGEPORT'S RAILROAD AVENUE RIGHT OF WAY. THESE DISTRIBUTION LINES WILL REMAIN UNCHANGED FROM EXISTING CONDITIONS.
5. TO COMPLY WITH UI STANDARD TRANSMISSION VEGETATION MANAGEMENT PLANS, UI REQUIRES VEGETATION MANAGEMENT IN THE AREA THAT IS 25'-0" FROM EACH STRUCTURE CENTER. IN LOCATIONS WHERE THE SURROUNDING ENVIRONMENT REQUIRES A PROPOSED STRUCTURE TO BE PLACED OUTSIDE OF THE CT DOT CORRIDOR, UI PROPOSES TO ACQUIRE A PERMANENT EASEMENT FROM ADJACENT LANDOWNERS TO ACHIEVE THESE CLEARANCES.



<sup>A</sup> THIS DISTANCE AVERAGES 48'-6" BUT CAN RANGE FROM 36'-0" TO 83'-0". REFER TO THE TABLE ON SHEET 20 FOR EXACT DISTANCES.

<sup>B</sup> THE HEIGHTS OF THE NEW 115-KV DOUBLE-CIRCUIT STRUCTURES VARY. REFER TO THE TABLE ON SHEET 20 FOR EXACT STRUCTURE HEIGHTS.



CADD Drawing, DO NOT REVISE MANUALLY.

ANSI B

PE Stamp  OWNER ENGINEER: _____ APPROVAL STAMP: _____  DRAWING PREPARED BY: _____ ACCEPTED BY OE: _____					<b>TYPICAL CROSS SECTION DIAGRAMS</b>  SHEET 15 OF 21  <b>UI 115KV RAILROAD PROJECT FAIRFIELD TO CONGRESS</b>			
0-0C	8/19/2022	WESTWOOD	ISSUE FOR REVIEW	MSP	DR.	ASW	SCALE: NTS	FILE:
0-0B	06/24/2022	WESTWOOD	ISSUE FOR REVIEW	MSP	CK.	MSP	NO.	REV.
0-0A	4/15/2022	WESTWOOD	ISSUE FOR REVIEW	MSP	APP.	---		0-0C
REV.	DATE	BY	DESCRIPTION	APP.	DATE:	4/15/2022	<b>XS-14</b>	

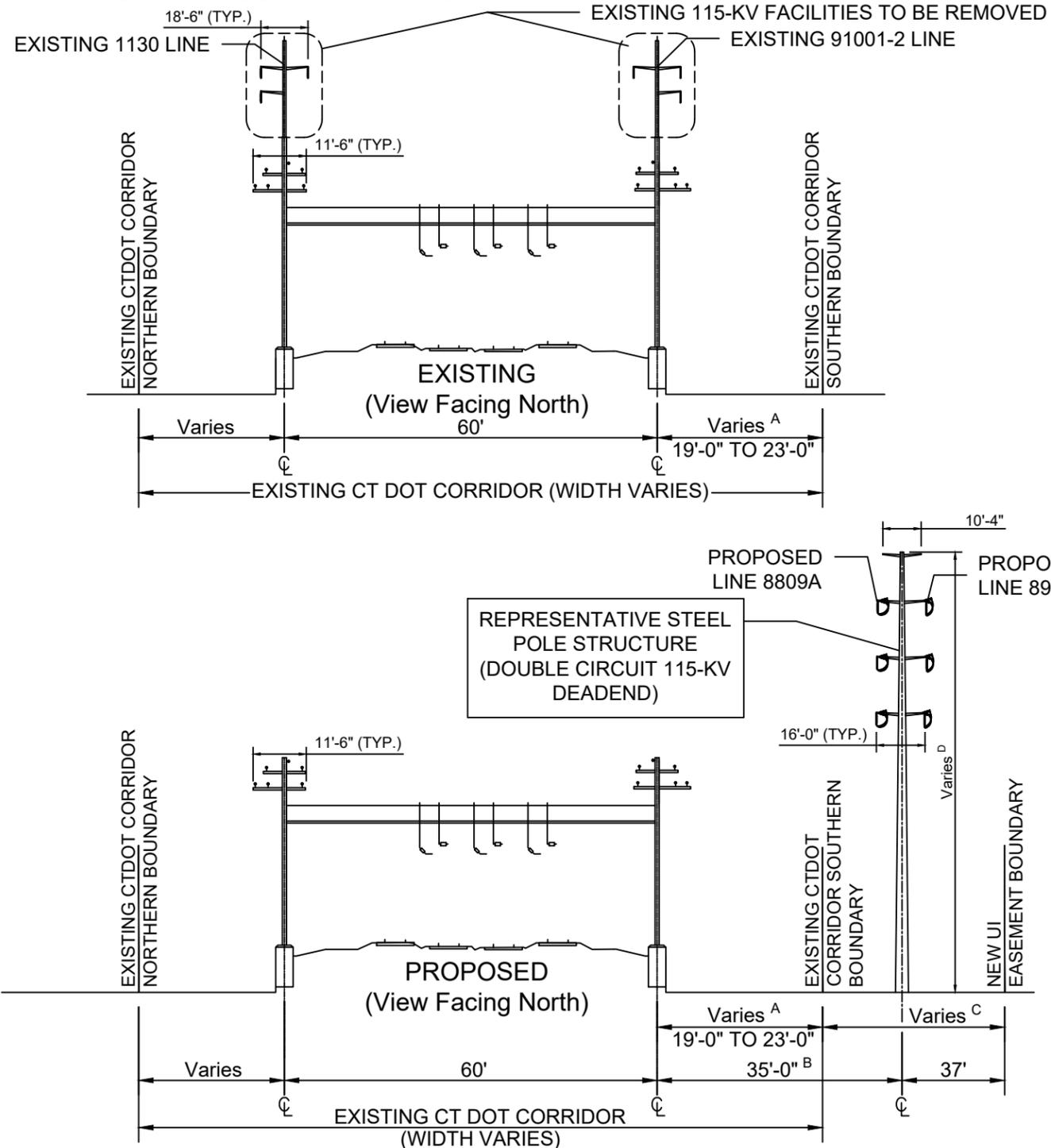




# CROSS SECTION 17

## PEQUONNOCK SUBSTATION TO CONGRESS STREET SUBSTATION (BRIDGEPORT)

### STR P779S TO STR P783S - 0.25 MILES



**NOTES:**

1. THE EXISTING CATENARY STRUCTURES SUPPORT METRO NORTH RAILROAD ELECTRICAL FACILITIES CONSISTING OF TWO (2) SIGNAL WIRES, 2 OR 3 FEEDER WIRES, AND ONE (1) AERIAL GROUND WIRE, BOTH ON THE NORTHERN AND SOUTHERN CATENARY SUPPORT COLUMNS.
2. THE CT DOT CORRIDOR CONTAINS FOUR (4) RAILROAD TRACKS IN THIS AREA OPERATED BY METRO NORTH RAILROAD.
3. A 15'-0" CLEARANCE MUST BE MAINTAINED BETWEEN THE NEW 115-KV CONDUCTORS AND THE EXISTING CATENARY STRUCTURES SO THAT METRO NORTH RAILROAD CAN MAINTAIN THEIR EQUIPMENT WITHOUT REQUIRING AN OUTAGE ON THE 115-KV FACILITIES.
4. TO COMPLY WITH UI STANDARD TRANSMISSION VEGETATION MANAGEMENT PLANS, UI REQUIRES VEGETATION MANAGEMENT IN THE AREA THAT IS 37'-0" FROM EACH STRUCTURE CENTER (29'-0" FROM CONDUCTOR ATTACHMENT POINT). IN LOCATIONS WHERE THE SURROUNDING ENVIRONMENT REQUIRES A PROPOSED STRUCTURE TO BE PLACED OUTSIDE OF THE CT DOT CORRIDOR, UI PROPOSES TO ACQUIRE A PERMANENT EASEMENT FROM ADJACENT LANDOWNERS TO ACHIEVE THESE CLEARANCES.
5. DISTANCE FROM NORTHERN CATENARY STRUCTURE TO CT DOT CORRIDOR NORTHERN BOUNDARY WILL REMAIN UNCHANGED FROM EXISTING CONDITIONS.

<sup>A</sup> THE WIDTH OF THE EXISTING CT DOT CORRIDOR IN THE PROJECT AREA IS HIGHLY VARIABLE. REFER TO ATTACHMENTS V2.3 AND V2.4: PROJECT MAPS AND DRAWINGS, FOR CT DOT CORRIDOR BOUNDARIES.

<sup>B</sup> THIS DISTANCE AVERAGES 35'-0" BUT CAN RANGE FROM 30'-0" TO 40'-0". REFER TO THE TABLE ON SHEET 21 FOR EXACT DISTANCES.

<sup>C</sup> THE WIDTH OF UI'S REQUIRED PERMANENT EASEMENT VARIES. REFER TO THE TABLE ON SHEET 21 FOR EXACT DISTANCES.

<sup>D</sup> THE HEIGHTS OF THE NEW 115-KV DOUBLE-CIRCUIT STRUCTURES VARIES. REFER TO THE TABLE ON SHEET 21 FOR EXACT STRUCTURE HEIGHTS.

CADD Drawing, DO NOT REVISE MANUALLY.

ANSI B

PE Stamp						<b>TYPICAL CROSS SECTION DIAGRAMS</b>					
						SHEET 18 OF 21					
						<b>UI 115KV RAILROAD PROJECT FAIRFIELD TO CONGRESS</b>					
						DR. ASW		SCALE: NTS		FILE:	
						CK. MSP		NO.		REV.	
						APP. ---		<b>XS-17</b>		0-0C	
						DATE: 4/15/2022					

REV.	DESCRIPTION	DATE	BY	CK	APP	OWNER ENGINEER:	APPROVAL STAMP:

REV.	DATE	BY	DESCRIPTION	APP.
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0-0B	06/24/2022	WESTWOOD	ISSUE FOR REVIEW	MSP
0-0A	4/15/2022	WESTWOOD	ISSUE FOR REVIEW	MSP

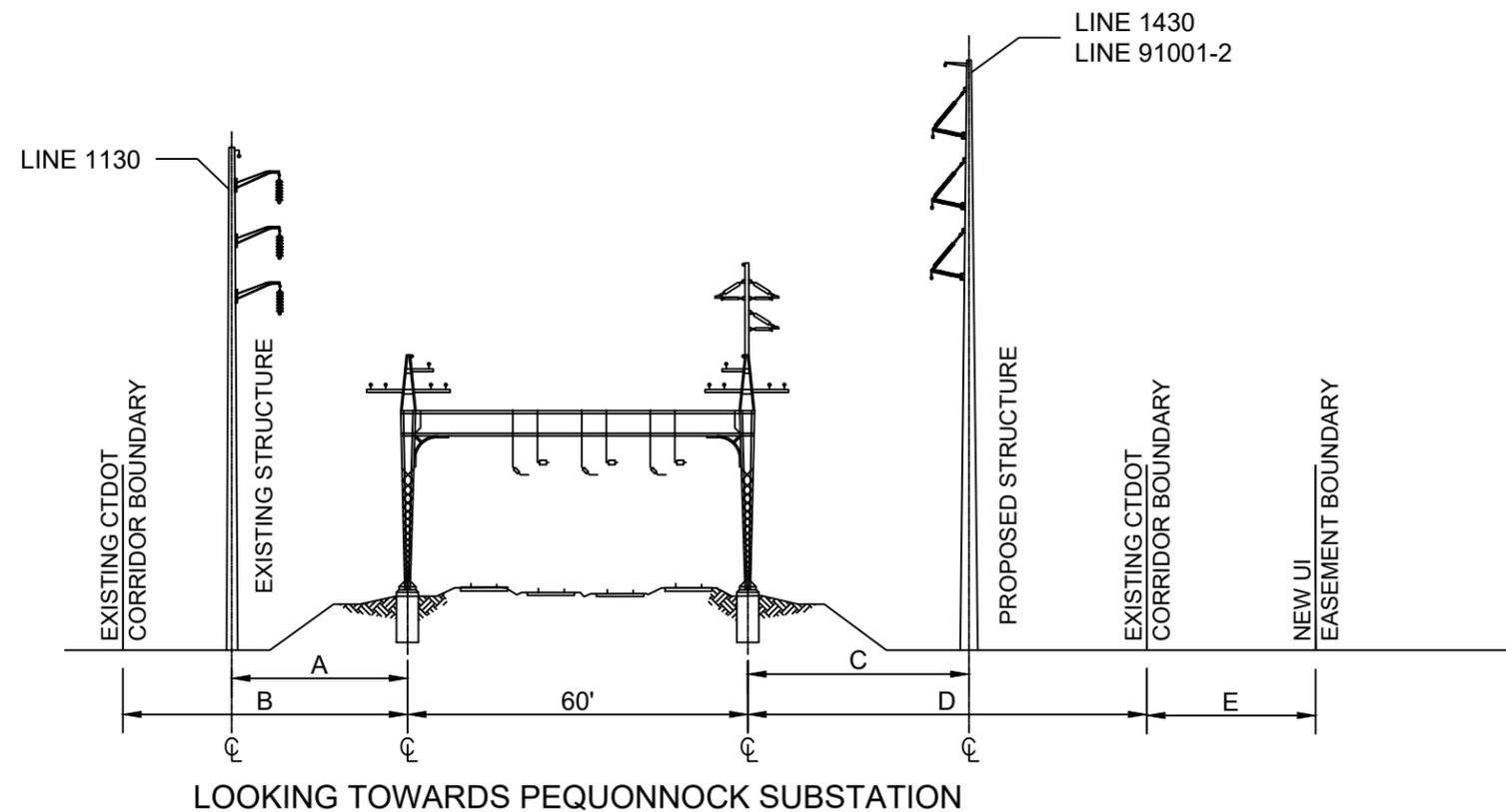
## LIST OF PROPOSED STRUCTURES BY CROSS SECTION REFERENCE

Structure	Cross Section #	Structure Height	A	B	C	D	E
P648S	1	100	4	7	19	39	-
P649S	1	105	4	7	18	39	-
P650S	1	105	3.5	6.5	19	39	-
P651S	1	110	4	7	21.5	39.5	-
P659S	1	135	0	7	9	48	-
P684S	1	120	3	100+	14	70	-
P685S	1	120	4	100+	10	76	-
P686S	1	125	3	100+	4	77	-
P655S	2	115	3	27	26	16.5	27.5
P656S	2	105	2.5	26.5	20	15.5	22.5
P664S	2	130	9	43	22	21	19
P699S	2	120	3	100+	19	18.5	18.5
P700S	2	110	3	100+	51	19	50
P701S	2	110	3	100+	25	18.5	24.5
P703S	2	105	3	100+	26	18.5	25.5
P708S	2	125	3	71	25	19	24
P657S	3	105	0	25	32	24.5	25.5
P681S	3	125	4	7	23	22	19
P661S	4	135	0	6	21	24.5	14.5
P668S	4	110	3	10	10	15	13
P669S	4	110	3	11	19	37	-
P671S	4	120	2	12	25	37.5	5.5
P679S	4	130	4	8	12	18.5	11.5
P682S	4	130	4	4	19	22	15
P663S	5	125	3	33.5	21	42.5	-
P666AS	5	110	0	100	25	100+	-
P688S	5	125	7	100+	13	100+	-
P706S	5	125	0	100+	29	69	-

Structure	Cross Section #	Structure Height	A	B	C	D	E
P665BS	6	125	6	42.5	23	35	6
P673S	6	125	3.5	8.5	18	24	12
P675S	6	115	3	8	17	30	5
P676S	6	105	3	8	17	30	5
P677S	6	105	4	8	16	18	16
P678S	6	120	4	8	10	18	10
P689S	6	135	5	16	16	22.5	11.5
P690S	6	120	4	16	18	21.5	14.5
P691S	6	110	4.5	19	18	22	14
P692S	6	120	4	20	17	21.5	13.5
P693S	6	125	4.5	20	18	21.5	14.5
P695S	6	125	4.5	19	20	20	18
P696S	6	125	4	19	15	19.5	13.5
P698S	6	130	3	100+	18	20	16
P704S	6	120	6	100+	63	68	13
P709S	6	110	3	71	12	20	10
P710AS	6	100	3	67	38	43	13
P710S	6	110	3	21	11	19	10
P711AS	6	100	0	67	15	18.5	14.5
P712S	6	105	3	67	12	18.5	11.5
P713S	6	110	0	67	10	19	9

Structure	Cross Section #	Structure Height	A	B	C	D	E
P716S	9	120	3	20	36	19	35
P721ES	9	130	2	19	16.5	19	15.5
P723S	9	125	2	19	17	6	29
P724S	9	105	2	28	17	18.5	16.5
P719S	10	115	3	19	10	100+	-
P725S	11	110	0	28	11	19	10
P726S	11	100	0	25	12	20	10
P727S	11	110	2	28	10	19	9
P728S	11	120	6	28	19	20	17
P730S	12	130	3	27	9	21	6
P733S	12	130	0	34	15	21	12

- A - DISTANCE FROM CATENARY STRUCTURE TO EXISTING POLE (NORTH SIDE)
- B - DISTANCE FROM CATENARY STRUCTURE TO EXISTING CORRIDOR BOUNDARY (NORTH SIDE)
- C - DISTANCE FROM CATENARY STRUCTURE TO PROPOSED POLE (SOUTH SIDE)
- D - DISTANCE FROM CATENARY STRUCTURE TO EXISTING CORRIDOR BOUNDARY (SOUTH SIDE)
- E - WIDTH OF PROPOSED UI EASEMENT (WHERE APPLICABLE)



CADD Drawing, DO NOT REVISE MANUALLY.

ANSI B

TYPICAL CROSS SECTION DIAGRAMS									
SHEET 19 OF 21									
UI 115KV RAILROAD PROJECT FAIRFIELD TO CONGRESS									
DR.		ASW		SCALE: NTS		FILE:		REV.	
CK.		MSP		NO.		XS DIMENSIONS TABLE 1		0-0C	
APP.		---		DATE: 4/15/2022					
REV.	DATE	BY	DESCRIPTION	APP.					
0-0C	8/19/2022	WESTWOOD	ISSUE FOR REVIEW	MSP					
0-0B	06/24/2022	WESTWOOD	ISSUE FOR REVIEW	MSP					
0-0A	4/15/2022	WESTWOOD	ISSUE FOR REVIEW	MSP					

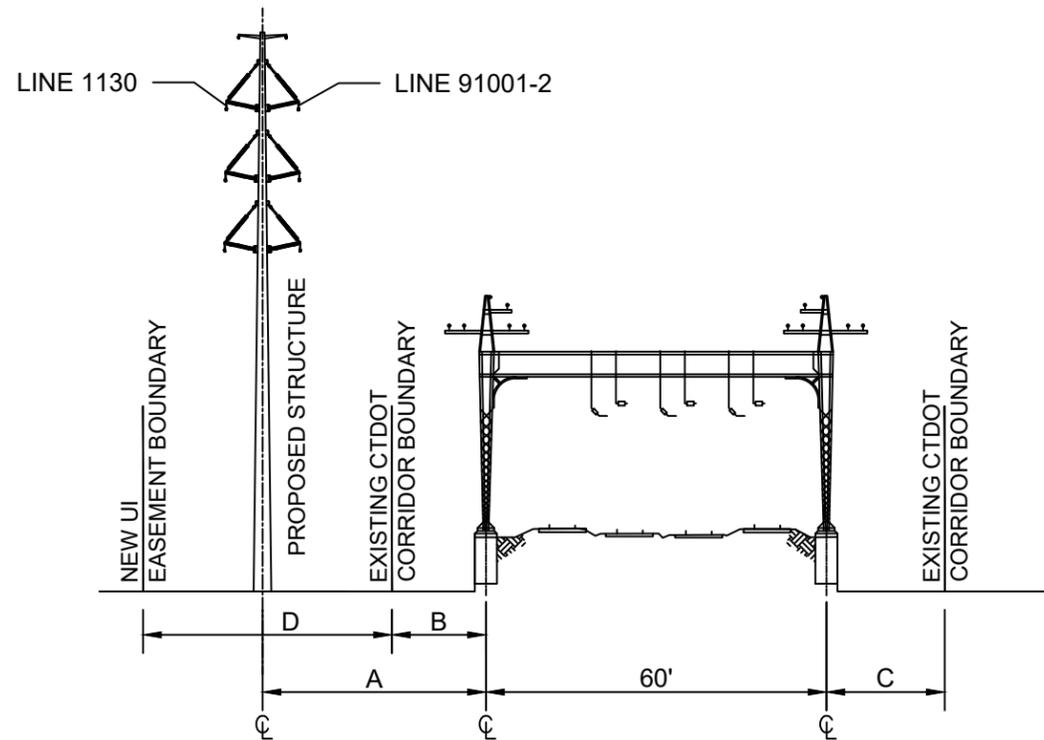
## LIST OF PROPOSED STRUCTURES BY CROSS SECTION REFERENCE

Structure	Cross Section #	Structure Height	A	B	C	D
P738N	13	100	48	0	0	80
P739N	13	95	48	0	0	80
P740N	13	110	48	0	0	80
P742N	13	105	48	0	0	80
P743N	13	100	47	0	0	79
P744EN	13	115	46	0	0	78
P744N	13	105	46	0	0	78
P745N	13	115	46	0	0	78

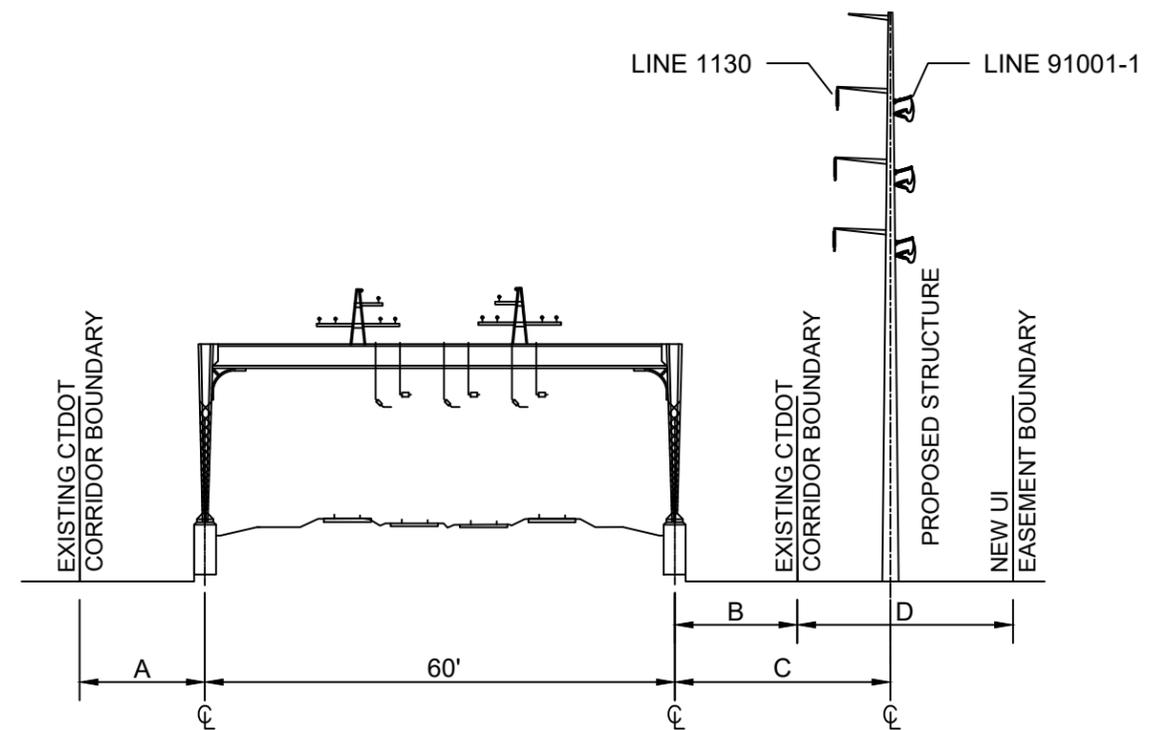
Structure	Cross Section #	Structure Height	A	B	C	D
P745S	14	115	0	0	52	77
P746S	14	115	0	0	51	76
P748S	14	115	0	0	42	67
P749S	14	115	0	0	38	63
P750S	14	115	0	0	48	73
P751S	14	120	0	0	49	74
P752S	14	130	0	0	37	62
P762S	14	125	0	0	50	75
P765AS	14	125	0	0	74	99

A - DISTANCE FROM CATENARY STRUCTURE TO PROPOSED POLE (NORTH SIDE)  
 B,C - THE RAILROAD CORRIDOR BOUNDARY IS AT THE EDGE OF THE CATENARY STRUCTURE  
 D - WIDTH OF PROPOSED UI EASEMENT

A,B - THE RAILROAD CORRIDOR BOUNDARY IS AT THE EDGE OF THE CATENARY STRUCTURE  
 C - DISTANCE FROM CATENARY STRUCTURE TO PROPOSED POLE (SOUTH SIDE)  
 D - WIDTH OF PROPOSED UI EASEMENT



LOOKING TOWARDS PEQUONNOCK SUBSTATION



LOOKING TOWARDS PEQUONNOCK SUBSTATION

CADD Drawing, DO NOT REVISE MANUALLY.

ANSI B

PE Stamp						TYPICAL CROSS SECTION DIAGRAMS					
SHEET 20 OF 21						UI 115KV RAILROAD PROJECT FAIRFIELD TO CONGRESS					
DRAWING PREPARED BY:      ACCEPTED BY OE:						DR. ASW		SCALE: NTS		FILE:	
REV.      DATE      BY      DESCRIPTION      APP.						CK. MSP		NO.		REV. 0-0C	
REV.      DATE      BY      DESCRIPTION      APP.						APP. ---		DATE: 4/15/2022		XS DIMENSIONS TABLE 2	

